

RD. ROAD	STATE	RED AND PRD NO.	FISCAL YEAR	SHEET NO.
0/31				
7	KY.			

NOTE:  
For General Notes, Standard Plans, Special Provision, and Bill  
of Incidental Material See Sheet NO2

BRIDGE CARRYING I71 OVER FISCHER SPUR R.R. SHEET 1 OF 2

**COMMONWEALTH OF KENTUCKY**  
**DEPARTMENT OF HIGHWAYS**  
**FRANKFORT**  
**COUNTY OF**  
**JEFFERSON**  
I 71 - OHIO STREET TO ZORN AVENUE  
LOUISVILLE - COVINGTON

STATION 355+33.19 PROJECT NO. 171-1(10)0  
BRIDGE DRAWING  
NUMBER 15936

TITLE SHEET

BRIDGE

# GENERAL NOTES

**SPECIFICATIONS:** Kentucky Department of Highways, Current Standard with Amendments.

**DESIGN LOAD:** Bridge designed for HS20-44 loading as specified in 1961 AASHTO Specifications or alternate loading of two 24 kip axles spaced 4 feet apart, whichever produces the greater stress.

**DESIGN CRITERIA:** For reinforced concrete:  
 $f_c = 3,000$  psi.  $u = 300$  psi./for E.  
 $f_s = 1,200$  psi.  $f_c = 1,000$  psi.  
 $u = 700$  psi.  $n = 10$   
for embedded

**PILE ALLOWABLE LOADS:** Piles are designed for a maximum load of 50 tons per pile. These maximums are for Group I loads with increases allowed for other loading groups.

**PILING:** Piling shall be driven to refusal or to sustain a minimum load of 75 tons per pile in accordance with the Specifications. Test piles shall be driven where designated on the plans to determine the length required. All test piles shall be accurately located so that they may be used in the finished structure.

**ALTERNATE TYPES OF PILES:** The Contractor shall use one of the following Alternate Types A through F throughout except that Types D and F will not be permitted in and below:

- Alternate A - 12" Precast Concrete Piles, Standard Drawing No. 12.
- Alternate B - 12" Cast-in-place Seamless or Welded Steel Shell Concrete Piles, Standard Drawing No. 120.
- Alternate C - 12" Central Butt Diameter Cast-in-place Steel Concrete Piles, Standard Drawing No. 121.
- Alternate D - 12" Minimum Nominal Butt Diameter Cast-in-place Steel Step-tapered Shell Concrete Piles, Standard Drawing No. 122.
- Alternate E - 12" Precast Prestressed Concrete Piles, Standard Drawing No. 123.
- Alternate F - 12" Minimum Nominal Butt Diameter Cast-in-place Steel Step-uniform Shell Concrete Piles, Standard Drawing No. 124.

**DRIVING PILES:** Cored pilot holes through the enhancement will be required for starting piles. The cost of this work is to be included in the unit price bid for driving piles.

**REINFORCEMENT:** Class "A" concrete is to be used throughout, except in piles. Class "B" concrete is to be used in piles.

**REINFORCEMENT:** Intermediate or hard grade reinforcement shall be used in accordance with AISC A513-SF for mill steel or A513-9T for mill steel.

Dimensions shown from face of concrete to bars are clear distances. Spacing of bars is from center to center of bars.

**BEVELED EDGES:** All exposed edges shall be beveled 7/8" unless otherwise shown.

**BAR ENCASED IN JOINT MATERIAL, PREMANAGED JOINT, PREMANAGED SEAL AND TAR PAPER:** The cost of these items is to be included in the unit price bid for Class "A" concrete.

**STRUCTURAL STEEL:** "Lump Sum Bid" for structural steel shall be full payment for all structural steel, bolts, washers, welding and welding materials, paint, and all labor and materials necessary to erect the steel in accordance with the plans and specifications.

**PLACING PILES:** CRUSHING SHALL BE PLACED IN ACCORDANCE WITH OTHER SPECIFICATIONS OF THE BIDDING.

**PAINT:** Surfaces of armor edge, not accessible after erection, shall be given two field coats of aluminum paint before erection. Shop paint shall not be applied to steel surfaces in contact with concrete.

**TAR PAPER:** Tar paper is to be commercial grade tar paper or roofing felt approximately 1/16" thick.

**CLEARANCE:** All falsework, bracing or forms shall have a minimum vertical clearance of 15 feet above the top of highest rail, and a minimum horizontal clearance of 8'-0" measured at right angles to the centerline of the nearest track.

**CONSTRUCTION NOTE:** The Contractor shall arrange the work so as not to interfere with the operation of the Fischer Packing Company at any time, unless the Contractor has made arrangements with the Fischer Packing Company. The Contractor shall arrange with the Fischer Packing Company to provide fire protection at the Contractor's expense as specified in the special provisions. Plans for the erection of the structure shall be submitted for approval to the Department of Highways and the Fischer Packing Company.

**CONCRETE FINISH:** Exterior face of exterior piers, abutments, soffit overhangs and wingwalls shall receive a rubbed surface finish. Piers shall receive an ordinary surface finish.

**UNSEED OIL PROTECTIVE PAINTING:** Protective coating shall be applied in accordance with the Special Provisions.

**WIND LOADS:** This structure is designed using wind loads based on a wind velocity of 50 M.P.H.

BILL OF INCIDENTAL MATERIAL			
1/2" X 1 1/2" Preformed Joint Seal	1" Preformed Expansion Joint Material	1/2" Preformed Expansion Joint Material	Tar Paper or Roofing Felt
Lin. Ft.	Sq. Ft.	Sq. Ft.	Sq. Ft.
120	400	400	100

NOTE: The "Bill of Incidental Material" is approximate only and the Contractor is responsible for furnishing enough material to complete the work in accordance with the Plans and Specifications.

## SPECIAL PROVISIONS

STANDARD DRAWING NO. 120 (12-11-61)  
LINKER OIL PROTECTIVE COATING (12-11-61)

## STANDARD DRAWING

- A8-1 Standard Armor Edge for Concrete
- 7-353 Preformed Joint Seal Details
- 1-351 Preformed Cork Expansion Joint Material
- 4-112 Standard Aluminum Handrail - 1 Pipe
- 4-113 Standard Steel Handrail - 1 Pipe
- 4-2 12" Precast Concrete Pile
- 4-20 12" Cast-in-place Concrete Pile
- 4-21 12" Cast-in-place Concrete Pile, Fluted Shell
- 4-22 12" Cast-in-place Pile, Corrugated Step-Taper Shell
- 4-23 12" Prestressed Precast Pile
- 4-24 12" Cast-in-place Pile, Corrugated Step-uniform Shell

BRIDGE CARRYING 171 OVER FISCHER SPUR R.R. SHEET 2 OF 10

**COMMONWEALTH OF KENTUCKY**  
**DEPARTMENT OF HIGHWAYS**  
**FRANKFORT**  
**COUNTY OF**  
**JEFFERSON**  
171 - OHIO STREET TO ZORN AVENUE  
LOUISVILLE - COVINGTON

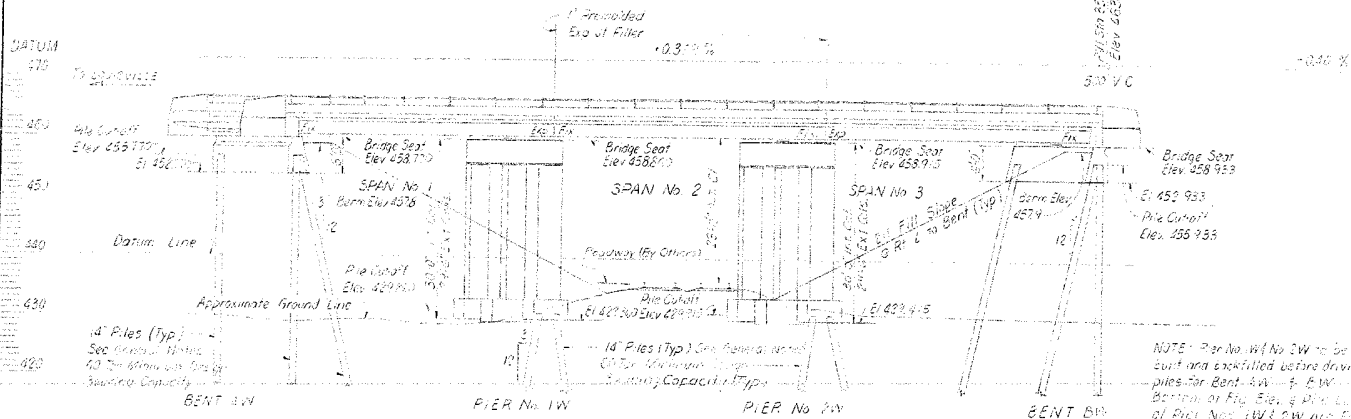
**ROAD**  
**STATION 355+33.19**  
**PROJECT NO. 171**

**DESIGNER**  
**CONTRACTOR**  
**NO.** 15936

GENERAL NOTES

**BRIDGE**

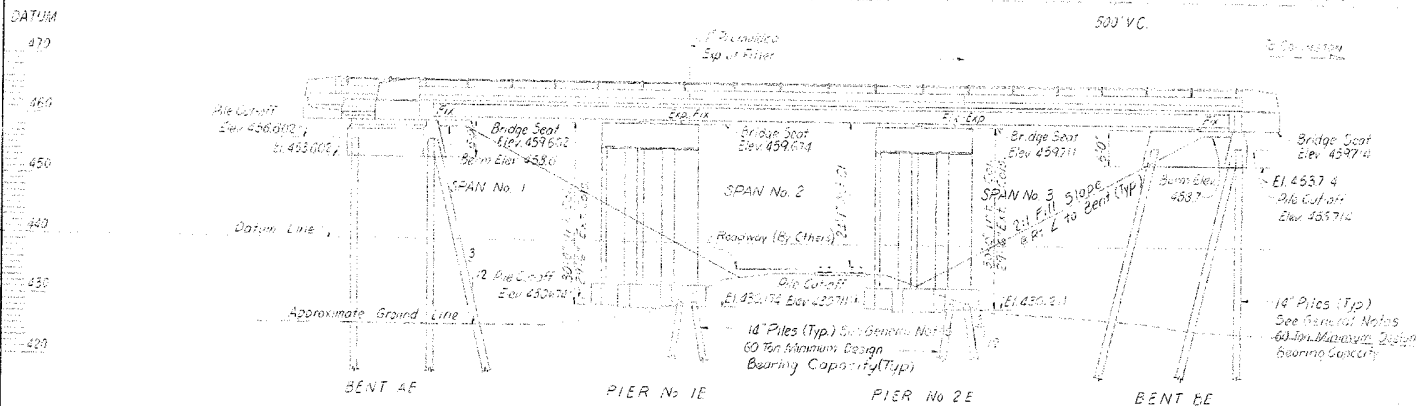
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### ELEVATION WESTBOUND BRIDGE

3 x 45' Simple RC G. Spans 13'10" Skew Left  
 H20-S16-44 Loading as specified in 1961 AASHTO (Tentative 1961)  
 Specifications or alternate loading at two 24-kip  
 wheels spaced 4 feet apart, whichever produces  
 the greater stress.  
 32'0" Highway 2'0" Safety Curbs, 43'4" Shoulders  
 at Bridge 2:1 Fill Slope  
 Structure on 12' Horizontal Curve

NOTE: Piers No. 1W & No. 2W to be  
 built and backfilled before driving  
 piles for Bent 1W & 2W.  
 Bottom of Fig. 316 & Pile Cutoff Elev.  
 of Pier Nos. 1W & 2W are For Ext. Cols.  
 Only. Elev's. For Int. Cols. Are  
 0.500' Lower.



### ELEVATION EASTBOUND BRIDGE

For description see Elevation North Bridge

NOTE: Piers No. 1E & No. 2E to be  
 built and backfilled before driving  
 piles for Bents 1E & 2E.  
 Bottom of Fig. 316 & Pile Cutoff Elev.  
 of Pier Nos. 1E & 2E are For Ext. Cols.  
 Only. Elev's. For Int. Cols. Are  
 0.500' Lower.

NOTES:  
 For General Notes see Sheet 2.  
 For Bridge Plan view see Sheet 3.

BRIDGE OVER FISCHER RAILROAD SPUR SHEET 4 OF 16

COMMONWEALTH OF KENTUCKY  
 DEPARTMENT OF HIGHWAYS

FRANKFORT

COUNTY OF

JEFFERSON

I 71 - OHIO STREET TO ZORN AVENUE

LOUISVILLE - COVINGTON

ROAD

STATION 355 + 33.18

PROJECT NO. 171

BRIDGE  
 NUMBER

15936

LAYOUT

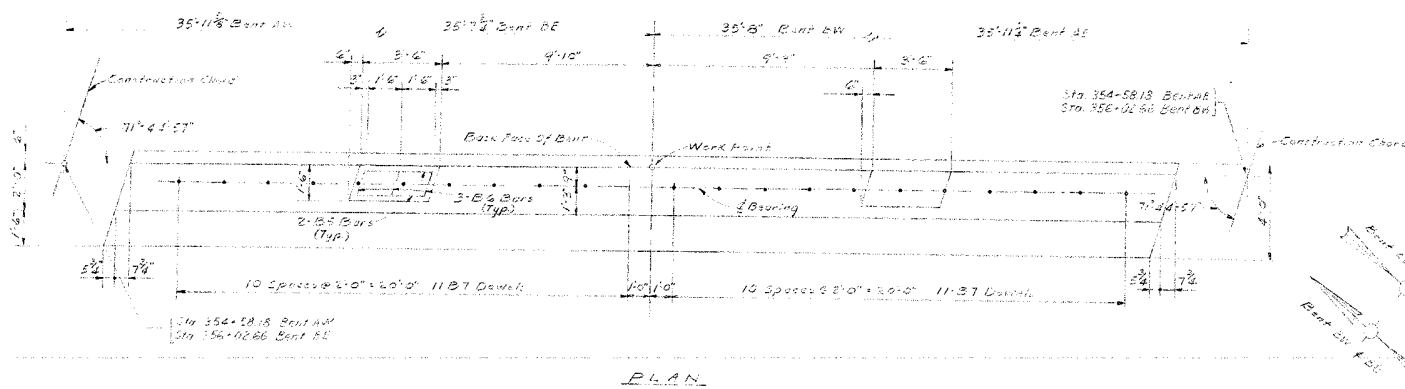
BRIDGE



REV.	DATE	BY	CHKD.	APP'D.
7	KY			

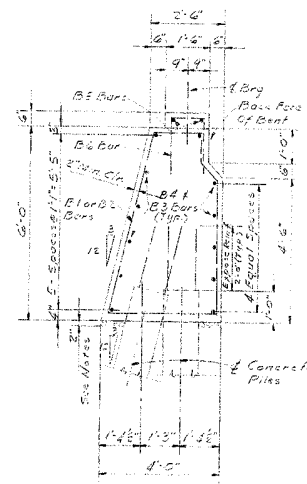
NOTES

For Location of Test Piles See Plans Sheet No. 13  
For Construction Details See Reinforcement Plan  
General Notes:  
1. Subgrade To 2' Below Final Elevation For Bottom  
Of Piles. Add 1" Layer Of 4# Crushed Stone On Gravel  
And Tamp Into Layer To Form A Subgrade. Place 1" 2"  
Concrete Mortar On 2" Bottom Of Subgrade. Place 1" 2"  
Forming For Casing May Be Placed As Soon As Mortar Has  
Set A Sufficient Time To Support Men And Forms  
Without Being Disturbed. This Method Is To Be Used  
Instead Of Bottom Of Casing Forms And The Cost Of  
Materials And Work Shall Be Included In To And Shall  
Be Included In The Unit Price Bid For Class A Concrete  
Do Not Backfill Above Bridge Seat Until Support  
Structure Is In Place.  
11. R Concrete Near Face, R.P. Concrete For Face, R.P.  
Concrete Back Face.



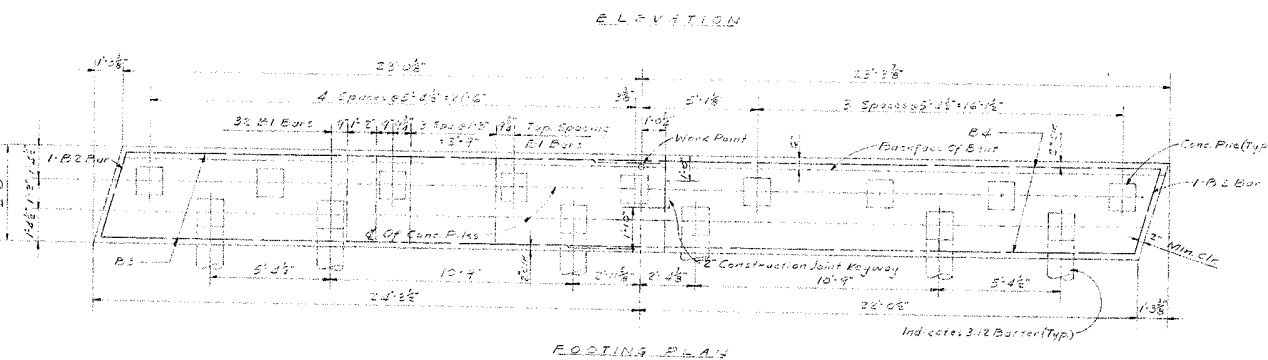
BILL OF REINFORCEMENT ONE BENT

Bar	Size	Number	Length Feet	Location	Feet	Notes
B1	1/2"	5	32	17	11	Cap
B2	1/2"	5	2	18	1	Cap
B3	3/4"	6	12	23	7	Cap
B4	3/4"	6	12	25	0	Cap
B5	3/4"	4	4	3	2	Shear Key
B6	2"	5	6	4	6	Shear Key
B7	3/4"	4	22	2	0	Dowels



ESTIMATE OF QUANTITIES (1 BENT)	
Class A Concrete	Cu. Yds 32.6
Steel Reinforcement	Lbs. 1580
Conc. Piles Furnished:	Lin. Ft. 825
Conc. Piles Driving	Lin. Ft. 925
Structural Exc. Common	Cu. Yds. 100
End Bent Backfill	Cu. Yds. 100

SECTION A-A



ELEVATION

BRIDGE OVER FISCHER RAILROAD SPUR SHEET 1 OF 15  
**COMMONWEALTH OF KENTUCKY**  
DEPARTMENT OF HIGHWAYS  
FRANKFORT  
COUNTY OF  
**JEFFERSON**  
171 OHIO STREET TO ZORN AVENUE  
LOUISVILLE - COVINGTON  
ROAD

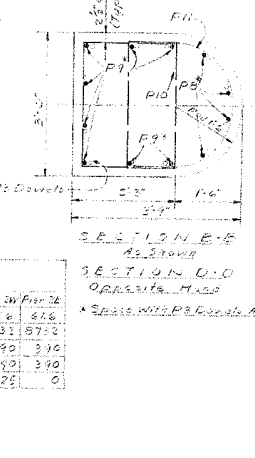
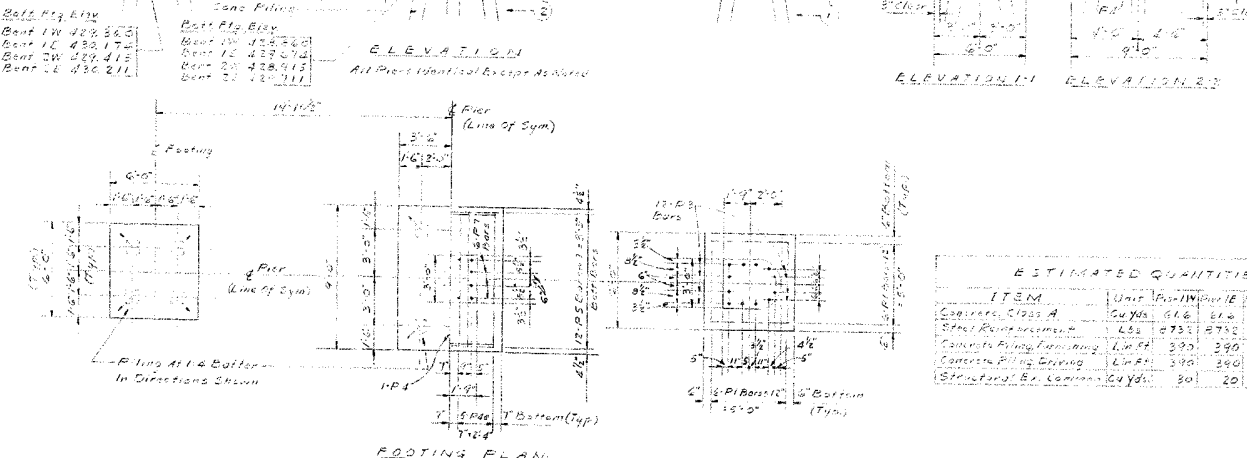
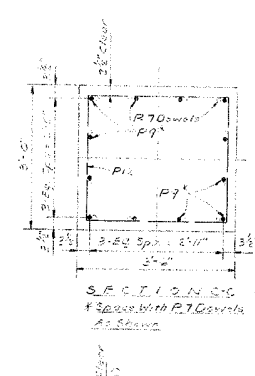
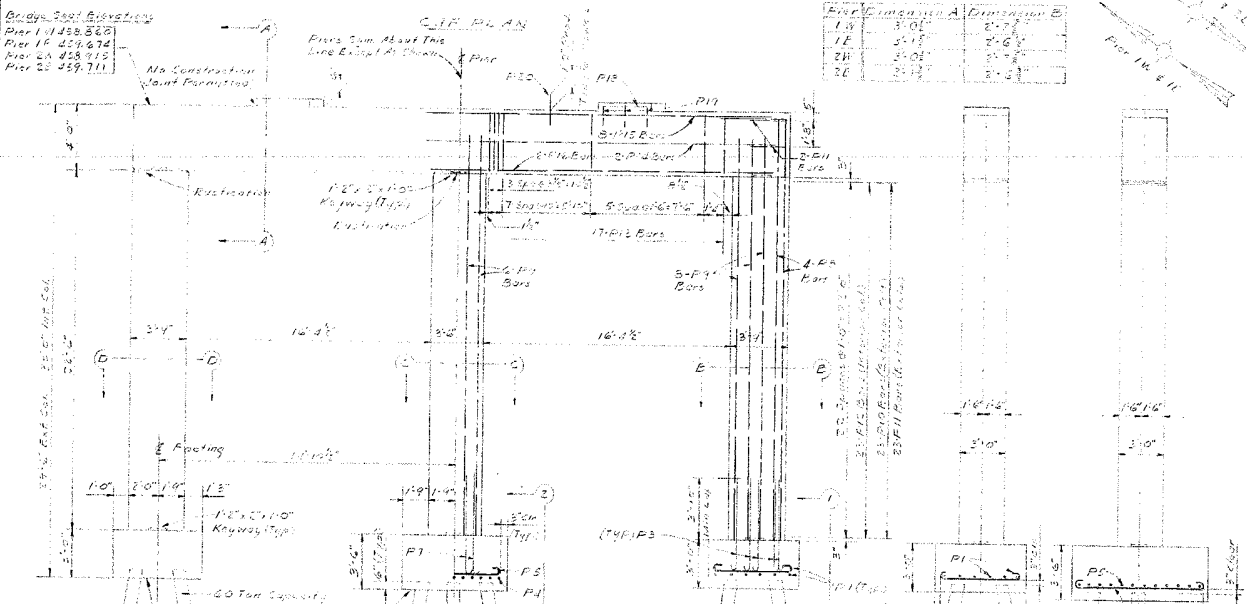
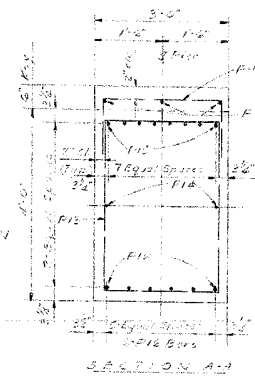
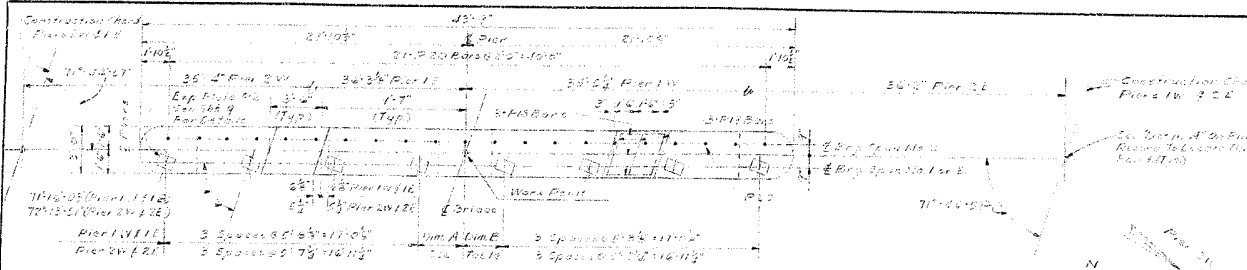
BENTS 1 & 2  
END BENT DETAILS

STATION 355+33.19 PROJECT NO. 171  
BRIDGE NUMBER DRAWING NO. 15736

**BRIDGE**

NO.	DATE	BY	CHKD.	APP'D.	REV.
7	KY.				

PIER REINFORCEMENT - PIER 1									
Bar No.	Size	Length	Location	Feet	Feet	Feet	Feet	Feet	Feet
P-1	2	5	24	6	Footings				
P-2	1	9	24	7	Footings				
P-3	2	7	11	12	Footings				
P-4	2	7	12	8	Footings				
P-5	1	9	12	8	Footings				
P-6	3	9	0	30	1	Ext. Cols.			
P-7	4	4	50	9	10	Ext. Cols.			
P-8	5	4	53	12	4	Int. Cols.			
P-9	5	4	54	13	4	Int. Cols.			
P-10	5	4	54	13	4	Int. Cols.			
P-11	5	4	54	13	4	Int. Cols.			
P-12	5	4	54	13	4	Int. Cols.			
P-13	5	4	54	13	4	Int. Cols.			
P-14	5	4	54	13	4	Int. Cols.			
P-15	5	4	54	13	4	Int. Cols.			
P-16	5	4	54	13	4	Int. Cols.			
P-17	5	4	54	13	4	Int. Cols.			
P-18	5	4	54	13	4	Int. Cols.			
P-19	5	4	54	13	4	Int. Cols.			
P-20	5	4	54	13	4	Int. Cols.			
P-21	5	4	54	13	4	Int. Cols.			
P-22	5	4	54	13	4	Int. Cols.			
P-23	5	4	54	13	4	Int. Cols.			
P-24	5	4	54	13	4	Int. Cols.			
P-25	5	4	54	13	4	Int. Cols.			
P-26	5	4	54	13	4	Int. Cols.			
P-27	5	4	54	13	4	Int. Cols.			
P-28	5	4	54	13	4	Int. Cols.			
P-29	5	4	54	13	4	Int. Cols.			
P-30	5	4	54	13	4	Int. Cols.			
P-31	5	4	54	13	4	Int. Cols.			
P-32	5	4	54	13	4	Int. Cols.			
P-33	5	4	54	13	4	Int. Cols.			
P-34	5	4	54	13	4	Int. Cols.			
P-35	5	4	54	13	4	Int. Cols.			
P-36	5	4	54	13	4	Int. Cols.			
P-37	5	4	54	13	4	Int. Cols.			
P-38	5	4	54	13	4	Int. Cols.			
P-39	5	4	54	13	4	Int. Cols.			
P-40	5	4	54	13	4	Int. Cols.			
P-41	5	4	54	13	4	Int. Cols.			
P-42	5	4	54	13	4	Int. Cols.			
P-43	5	4	54	13	4	Int. Cols.			
P-44	5	4	54	13	4	Int. Cols.			
P-45	5	4	54	13	4	Int. Cols.			
P-46	5	4	54	13	4	Int. Cols.			
P-47	5	4	54	13	4	Int. Cols.			
P-48	5	4	54	13	4	Int. Cols.			
P-49	5	4	54	13	4	Int. Cols.			
P-50	5	4	54	13	4	Int. Cols.			
P-51	5	4	54	13	4	Int. Cols.			
P-52	5	4	54	13	4	Int. Cols.			
P-53	5	4	54	13	4	Int. Cols.			
P-54	5	4	54	13	4	Int. Cols.			
P-55	5	4	54	13	4	Int. Cols.			
P-56	5	4	54	13	4	Int. Cols.			
P-57	5	4	54	13	4	Int. Cols.			
P-58	5	4	54	13	4	Int. Cols.			
P-59	5	4	54	13	4	Int. Cols.			
P-60	5	4	54	13	4	Int. Cols.			
P-61	5	4	54	13	4	Int. Cols.			
P-62	5	4	54	13	4	Int. Cols.			
P-63	5	4	54	13	4	Int. Cols.			
P-64	5	4	54	13	4	Int. Cols.			
P-65	5	4	54	13	4	Int. Cols.			
P-66	5	4	54	13	4	Int. Cols.			
P-67	5	4	54	13	4	Int. Cols.			
P-68	5	4	54	13	4	Int. Cols.			
P-69	5	4	54	13	4	Int. Cols.			
P-70	5	4	54	13	4	Int. Cols.			
P-71	5	4	54	13	4	Int. Cols.			
P-72	5	4	54	13	4	Int. Cols.			
P-73	5	4	54	13	4	Int. Cols.			
P-74	5	4	54	13	4	Int. Cols.			
P-75	5	4	54	13	4	Int. Cols.			
P-76	5	4	54	13	4	Int. Cols.			
P-77	5	4	54	13	4	Int. Cols.			
P-78	5	4	54	13	4	Int. Cols.			
P-79	5	4	54	13	4	Int. Cols.			
P-80	5	4	54	13	4	Int. Cols.			
P-81	5	4	54	13	4	Int. Cols.			
P-82	5	4	54	13	4	Int. Cols.			
P-83	5	4	54	13	4	Int. Cols.			
P-84	5	4	54	13	4	Int. Cols.			
P-85	5	4	54	13	4	Int. Cols.			
P-86	5	4	54	13	4	Int. Cols.			
P-87	5	4	54	13	4	Int. Cols.			
P-88	5	4	54	13	4	Int. Cols.			
P-89	5	4	54	13	4	Int. Cols.			
P-90	5	4	54	13	4	Int. Cols.			
P-91	5	4	54	13	4	Int. Cols.			
P-92	5	4	54	13	4	Int. Cols.			
P-93	5	4	54	13	4	Int. Cols.			
P-94	5	4	54	13	4	Int. Cols.			
P-95	5	4	54	13	4	Int. Cols.			
P-96	5	4	54	13	4	Int. Cols.			
P-97	5	4	54	13	4	Int. Cols.			
P-98	5	4	54	13	4	Int. Cols.			
P-99	5	4	54	13	4	Int. Cols.			
P-100	5	4	54	13	4	Int. Cols.			



ESTIMATED QUANTITIES				
ITEM	Unit	Quantity	Price	Total
Concrete Class A	Cu Yds.	616	616	616
Steel Reinforcement	Lbs.	8731	8731	8731
Concrete Paving	Sq. Yds.	390	390	390
Concrete Paving Gravel	Sq. Yds.	390	390	390
Structural Br. Common	Cu Yds.	301	20	25

PIER DETAILS

BRIDGE OVER FISCHER RAILROAD SPUR SHEET 7 OF 12

**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 FRANKFORT  
 COUNTY OF  
**JEFFERSON**  
 171 OHIO STREET TO ZORN AVENUE  
 LOUISVILLE - COVINGTON  
 ROAD

STATION 355+33.19 PROJECT NO. I 71

BRIDGE NUMBER DRAWING INDEX

NO. 15426

**BRIDGE**

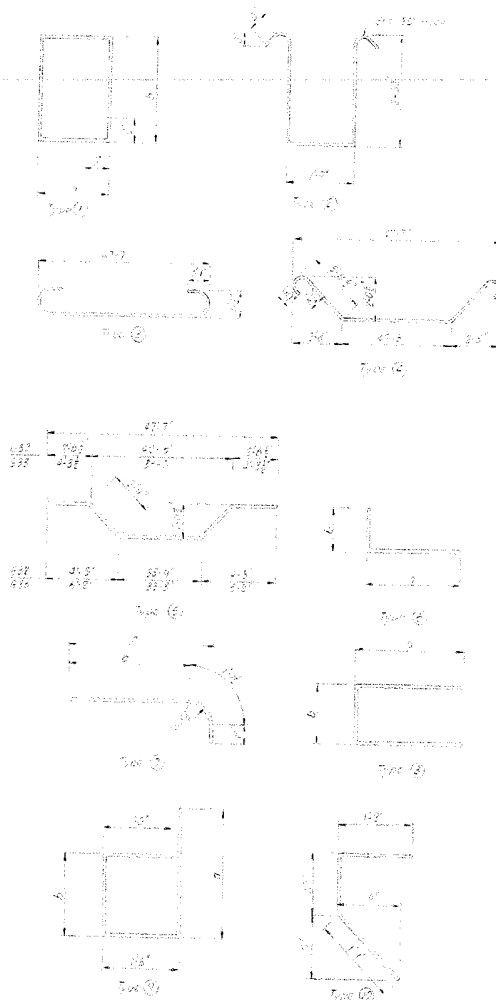




**BRIDGE**

## BILL OF REINFORCEMENT

## ESTIMATE OF QUANTITIES

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1765.  
 1765. 1765. 1765. 1765. 1765.

BRIDGE CARRYING 171 OVER FISCHER SPUR R.R. SHEET 10 OF 11

**COMMONWEALTH OF KENTUCKY**  
**DEPARTMENT OF HIGHWAYS**  
**FRANKFORT**  
**COUNTY OF**  
**JEFFERSON**  
I 71 - OHIO STREET TO ZORN AVENUE  
LOUISVILLE-COVINGTON

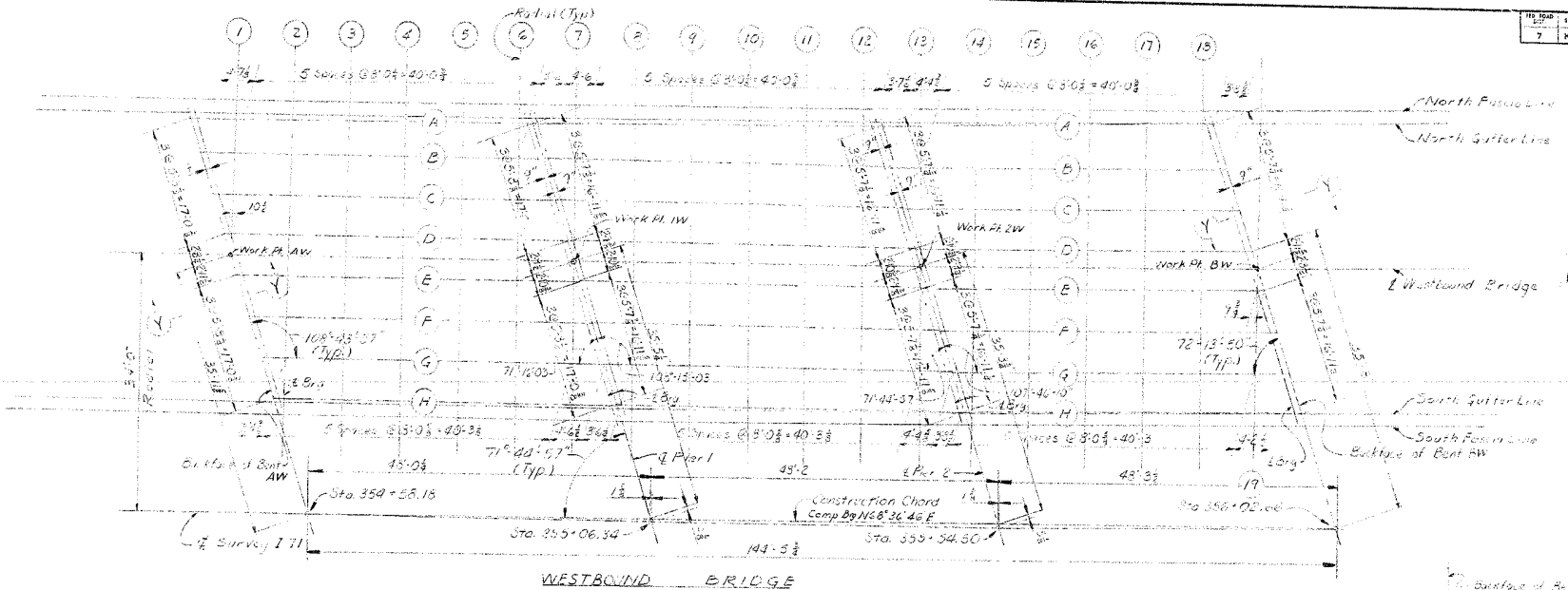
STATION 355 + 33.19 PROJECT NO. 17

BRIDGE NUMBER	DRAWING 15436	INDEX
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## SUPERSTRUCTURE

BRIDGE

Survey  
 Station 355+33.19  
 L = 22' 30" 00"  
 D = 1' 00"  
 T = 134.925  
 E = 154.73  
 L = 2650.15  
 F1310 360+3170



FASCIA OFFSETS

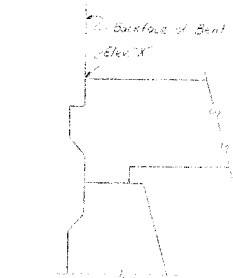
Station	Left	Right
1	0.00	0.00
2	0.00	0.00
3	0.00	0.00
4	0.00	0.00
5	0.00	0.00
6	0.00	0.00
7	0.00	0.00
8	0.00	0.00
9	0.00	0.00
10	0.00	0.00
11	0.00	0.00
12	0.00	0.00
13	0.00	0.00
14	0.00	0.00
15	0.00	0.00
16	0.00	0.00
17	0.00	0.00
18	0.00	0.00
19	0.00	0.00
20	0.00	0.00

TABLE OF ELEVATIONS

LOCATION	W GUTTER	BEAM B	BEAM C	BEAM D	BEAM E	BEAM F	BEAM G	S GUTTER
Backface Bent AW	462.080	462.185	462.337	462.494	462.647	462.800	462.953	463.106
Pier 1 W Span 1	462.110	462.210	462.310	462.410	462.510	462.610	462.710	462.810
Pier 1 W Span 2	462.140	462.240	462.340	462.440	462.540	462.640	462.740	462.840
Pier 2 W Span 1	462.170	462.270	462.370	462.470	462.570	462.670	462.770	462.870
Pier 2 W Span 2	462.200	462.300	462.400	462.500	462.600	462.700	462.800	462.900
Backface Bent BW	462.230	462.330	462.430	462.530	462.630	462.730	462.830	462.930

TABLE OF ELEVATIONS

SECTION	W GUTTER	BEAM B	BEAM C	BEAM D	BEAM E	BEAM F	BEAM G	S GUTTER
1	462.050	462.150	462.250	462.350	462.450	462.550	462.650	462.750
2	462.080	462.180	462.280	462.380	462.480	462.580	462.680	462.780
3	462.110	462.210	462.310	462.410	462.510	462.610	462.710	462.810
4	462.140	462.240	462.340	462.440	462.540	462.640	462.740	462.840
5	462.170	462.270	462.370	462.470	462.570	462.670	462.770	462.870
6	462.200	462.300	462.400	462.500	462.600	462.700	462.800	462.900
7	462.230	462.330	462.430	462.530	462.630	462.730	462.830	462.930
8	462.260	462.360	462.460	462.560	462.660	462.760	462.860	462.960
9	462.290	462.390	462.490	462.590	462.690	462.790	462.890	462.990
10	462.320	462.420	462.520	462.620	462.720	462.820	462.920	463.020
11	462.350	462.450	462.550	462.650	462.750	462.850	462.950	463.050
12	462.380	462.480	462.580	462.680	462.780	462.880	462.980	463.080
13	462.410	462.510	462.610	462.710	462.810	462.910	463.010	463.110
14	462.440	462.540	462.640	462.740	462.840	462.940	463.040	463.140
15	462.470	462.570	462.670	462.770	462.870	462.970	463.070	463.170
16	462.500	462.600	462.700	462.800	462.900	463.000	463.100	463.200
17	462.530	462.630	462.730	462.830	462.930	463.030	463.130	463.230
18	462.560	462.660	462.760	462.860	462.960	463.060	463.160	463.260
19	462.590	462.690	462.790	462.890	462.990	463.090	463.190	463.290

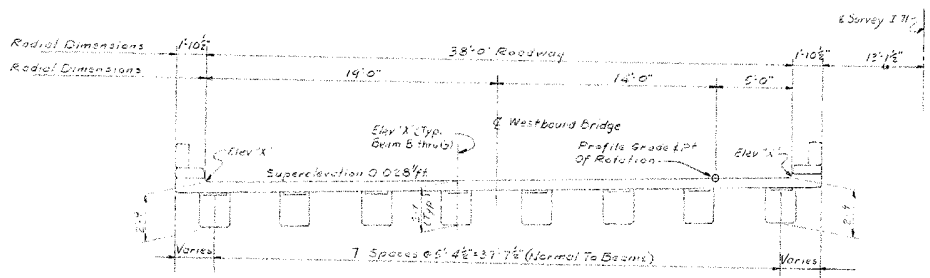


SECTION Y-Y

GUTTER LINE OFFSETS

Offset Line	Gutter A	Gutter B
Backface Bent AW	4.0	4.0
Pier 1 W Span 1	4.0	4.0
Pier 1 W Span 2	4.0	4.0
Pier 2 W Span 1	4.0	4.0
Pier 2 W Span 2	4.0	4.0
Backface Bent BW	4.0	4.0

\* Distances measured along Survey Lines

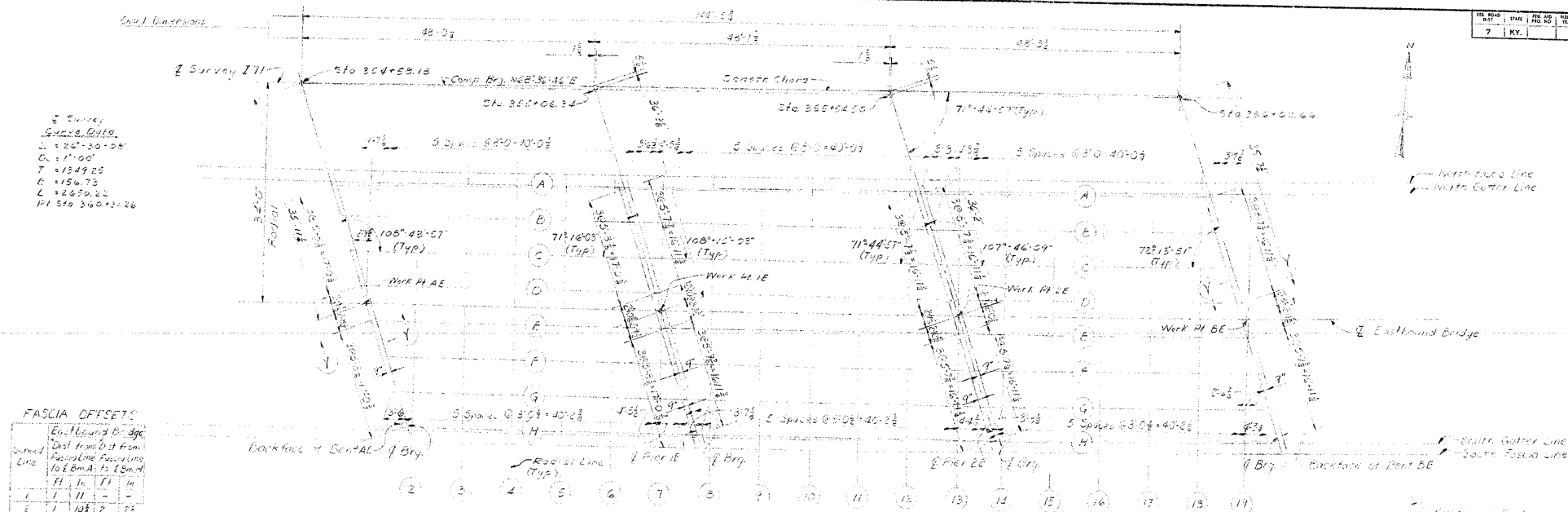


TYPICAL SECTION

NOTE:  
 Construction Elevations show elevations at top of sub. These elevations include the deflections due to dead load and are to be maintained while falsework is in place

WESTBOUND BRIDGE  
 CONSTRUCTION ELEVATIONS

### Local Divergence



EASTBOUND BRIDGE

FASCIA OFFSETS					
Line	Eastman Kodak				
	Dist. from front of				
	horizontal fascicle				
	F1	F2	F3	F4	F5
1	1	11	-	-	-
2	1	10	2	2	2
3	1	10 1/2	2	2	2
4	1	11	2	2	2
5	1	11 1/2	2	2	2
6	1	11 1/2	2	2	2
7	2	2	2	2	1 1/2
8	2	2	2	2	1 1/2
9	2	2	2	2	1 1/2
10	2	2	2	2	1 1/2
11	2	2	2	2	1 1/2
12	2	2	2	2	1 1/2
13	2	2	2	2	1 1/2
14	2	2	2	2	1 1/2
15	2	2	2	2	1 1/2
16	2	2	2	2	1 1/2
17	2	2	2	2	1 1/2
18	2	2	2	2	1 1/2
19	-	-	1	1	1 1/2

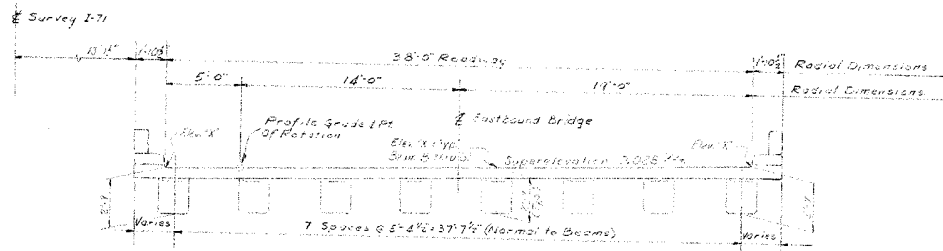
\*Distance measured along Street Edge

TABLE OF ELEVATIONS									
ELEVATION 'X'									
LOCATION	NIGHTER	BEAM E	BEAM S	BEAM D	BEAM C	BEAM F	BEAM S	NIGHTER	
Griffin Bent 1E	428.205	428.003	428.161	428.315	428.468	428.621	428.776	428.929	
Griffin 1E Spine 1	428.214	428.000	428.325	428.395	428.593	428.791	428.990	429.188	
Griffin 1E Spine 2	428.214	428.251	428.555	428.336	428.655	428.974	429.292	429.611	
Griffin 2E Spine 1	428.214	428.178	428.270	428.471	428.572	428.714	428.877	429.009	
Griffin 2E Spine 2	428.214	428.178	428.271	428.476	428.575	428.775	428.974	429.087	
Backhoe Bent 1E	428.194	428.322	428.476	428.498	428.593	428.722	428.874	429.005	

TABLE OF ELEVATIONS									
ELEVATION "X"									
SECTION	N. GUTTER	BEAM B	BEAM C	BEAM D	BEAM E	BEAM F	BEAM G	GUTTER	
1	462.525	463.087	463.78	463.214					
2	462.916	463.506	463.106	462.858	463.495	463.144	463.741	463.144	
3	462.186	463.083	463.232	463.549	463.514	463.476	463.822	463.478	
4	462.181	463.778	462.246	463.217	463.545	463.111	463.847	463.255	
5	462.157	463.074	463.180	463.420	463.504	462.577	463.521	464.251	
6	462.515	462.022	462.229	463.514	463.555	463.725	463.351	464.002	
7	462.787	463.076	463.342	463.337	462.735	463.411	463.241	464.312	
8	461.797	463.123	462.270	463.417	463.263	463.706	463.554	463.066	
9	462.704	462.181	463.741	463.439	463.493	463.132	463.331	463.035	
10	462.711	463.141	463.141	463.500	463.201	463.120	463.100	463.500	
11	462.702	463.141	462.215	463.469	463.201	463.755	463.116	463.000	
12	462.702	463.141	463.531	462.797	463.571	463.747	463.710	463.531	
13	462.777	463.751	463.271	463.242	463.575	463.921	462.935	463.751	
14	462.235	463.152	463.230	462.446	463.219	463.757	463.353	463.235	
15	462.109	462.465	463.214	462.545	463.611	463.708	463.170	462.056	
16	463.007	463.163	463.517	463.465	462.643	463.769	463.717	464.371	
17	462.712	463.183	463.307	463.461	462.543	462.767	463.712	463.712	
18	462.903	463.51	463.257	463.342	463.597	462.755	463.706	463.257	
19					463.574	462.720	463.706	463.574	

25. TION 8-Y

Offset Lin.	Grade A South butter	Grade A North butter
Backface Cent AE	3	2
4 Plier 1E Span	1	2
6 Plier 1E Span 2	2	3
8 Plier 1E Span 2	2	3
6 Plier 2E Span 3	3	1
Backface Cent BE	3	3



TYPICAL SECTION

*NOTE:*

Construction Elevations show elevations of top of slab. These elevations include the deflections due to dead load and are to be maintained while top work is in place.

EASTBOUND BRIDGE  
CONSTRUCTION ELEVATIONS

BRIDGE OVER FISCHER RAILROAD SPUR SHEET 12 OF 15

COMMONWEALTH OF KENTUCKY

DEPARTMENT OF HIGHWAYS

FRANKFORT  
COUNTY OF  
JEFFERSON  
I 71 - OHIO STREET TO ZORN AVENUE  
LOUISVILLE - COVINGTON  
ROAD

**STATION** 355 + 33.19

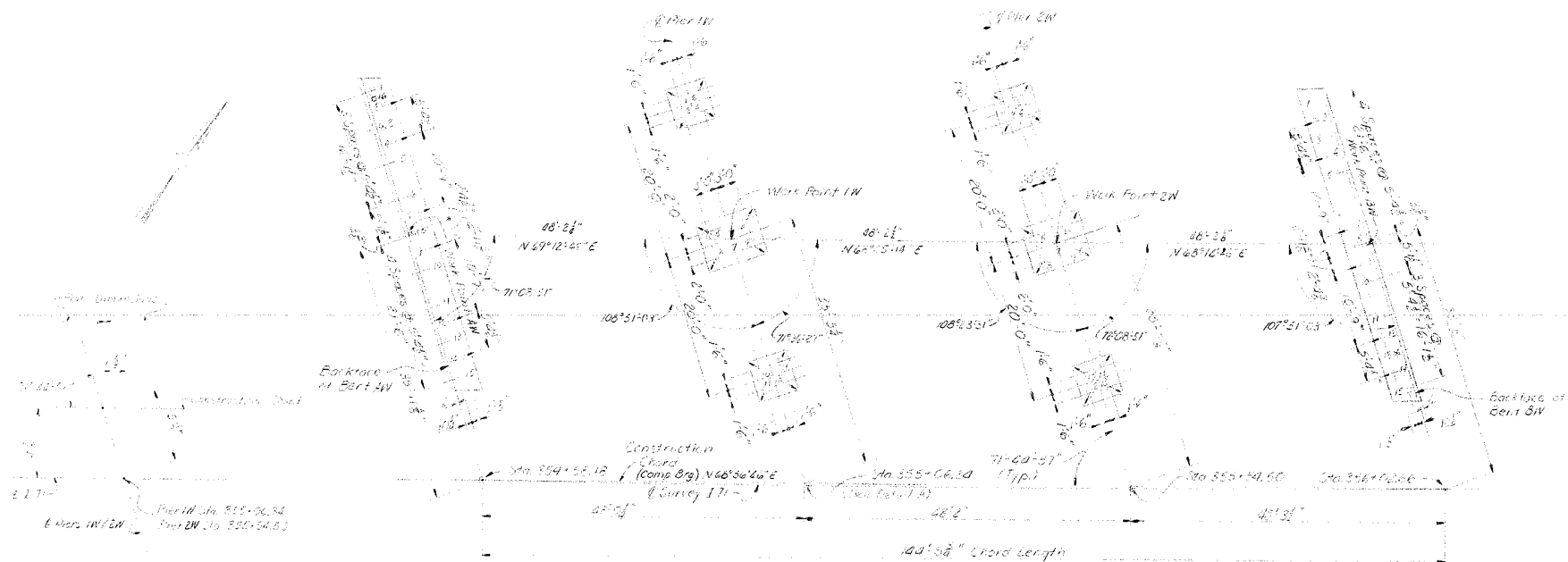
BRIDGE  
NUMBER

PROJECT NO. 171

**DRAWING**

.....

BRIDGE



PILING PLAN - WESTBOUND BRIDGE

Notes:  
 This pile record does not replace other records of piles required to be kept and submitted by the Resident Engineer. After all piles have been driven, the Resident Engineer shall record the tip of pile elevation, as driven, the length of pile in place, and the calculated bearing capacity of each pile. He shall return one blueprint of this sheet to the Director of Bridges, so that the data may be recorded on the original plans. Lengths of piles in place shown hereon are the actual lengths of piles in the finished structure below the cut-off elevation and are not necessarily pay items.

END BENT 4W				
Pile No.	Cut-off Elevation	Tip of Pile (as driven)	Length of Pile in Place	Bearing Capacity (tons)
1	455.770	Bottom out (L.V.)		
2		400.55	54.98	70
3		375.66	55.58	70
4		409.12	52.65	75
5		398.70	52.50	85
6		409.00	51.75	90
7		408.86	49.42	95
8		398.02	51.75	90
9		397.92	51.61	85
10		407.79	49.83	90
11		408.20	47.71	85
12		406.05	51.25	85
13		407.62	48.25	85
14		406.86	50.92	85
15	455.770	408.190	47.58	85
16	455.770	398.33	47.83	85

PIER 1W				
Pile No.	Cut-off Elevation	Tip of Pile (as driven)	Length of Pile in Place	Bearing Capacity (tons)
15	429.850	395.23	34.62	75
16		395.56	34.42	80
17		397.12	32.73	85
18	429.860	394.21	35.65	85
19	429.860	394.21	35.65	85
20		394.66	34.71	85
21		394.76	34.40	85
22		394.76	34.40	85
23		394.76	34.40	85
24	429.860	394.76	34.40	85
25	429.860	394.76	34.40	85
26		394.76	34.40	85
27		394.76	34.40	85
28		394.76	34.40	85
29		394.76	34.40	85
30	429.860	394.76	34.40	85

PIER 2W				
Pile No.	Cut-off Elevation	Tip of Pile (as driven)	Length of Pile in Place	Bearing Capacity (tons)
31	429.915	397.16	32.75	85
32		398.51	31.36	85
33		398.06	31.85	85
34	429.915	397.06	32.85	85
35	429.915	397.06	32.85	85
36	429.915	397.06	32.85	85
37		397.06	32.85	85
38		397.06	32.85	85
39		397.06	32.85	85
40	429.915	397.06	32.85	85
41		397.06	32.85	85
42		397.06	32.85	85
43	429.915	397.06	32.85	85
44		397.06	32.85	85
45		397.06	32.85	85
46		397.06	32.85	85
47		397.06	32.85	85
48		397.06	32.85	85
49		397.06	32.85	85
50	429.915	397.06	32.85	85

END BENT 3W				
Pile No.	Cut-off Elevation	Tip of Pile (as driven)	Length of Pile in Place	Bearing Capacity (tons)
51	455.933	410.88	45.05	90
52		410.88	45.05	90
53		409.00	46.85	90
54		408.81	47.59	90
55		408.75	47.18	85
56		408.13	47.80	92
57		408.44	48.95	102
58		406.19	49.74	10
59		406.36	48.42	15
60		406.70	47.03	85
61		408.38	47.55	90
62		408.82	46.56	90
63		408.36	47.57	90
64		408.23	48.19	95
65	455.933	408.47	46.76	90

- Indicates direction of "A" batter  
 \* Indicates test pile for length  
 (Estimated length: 35' @ Piers & 60' @ End Bents)

WESTBOUND BRIDGE  
 PILE RECORD

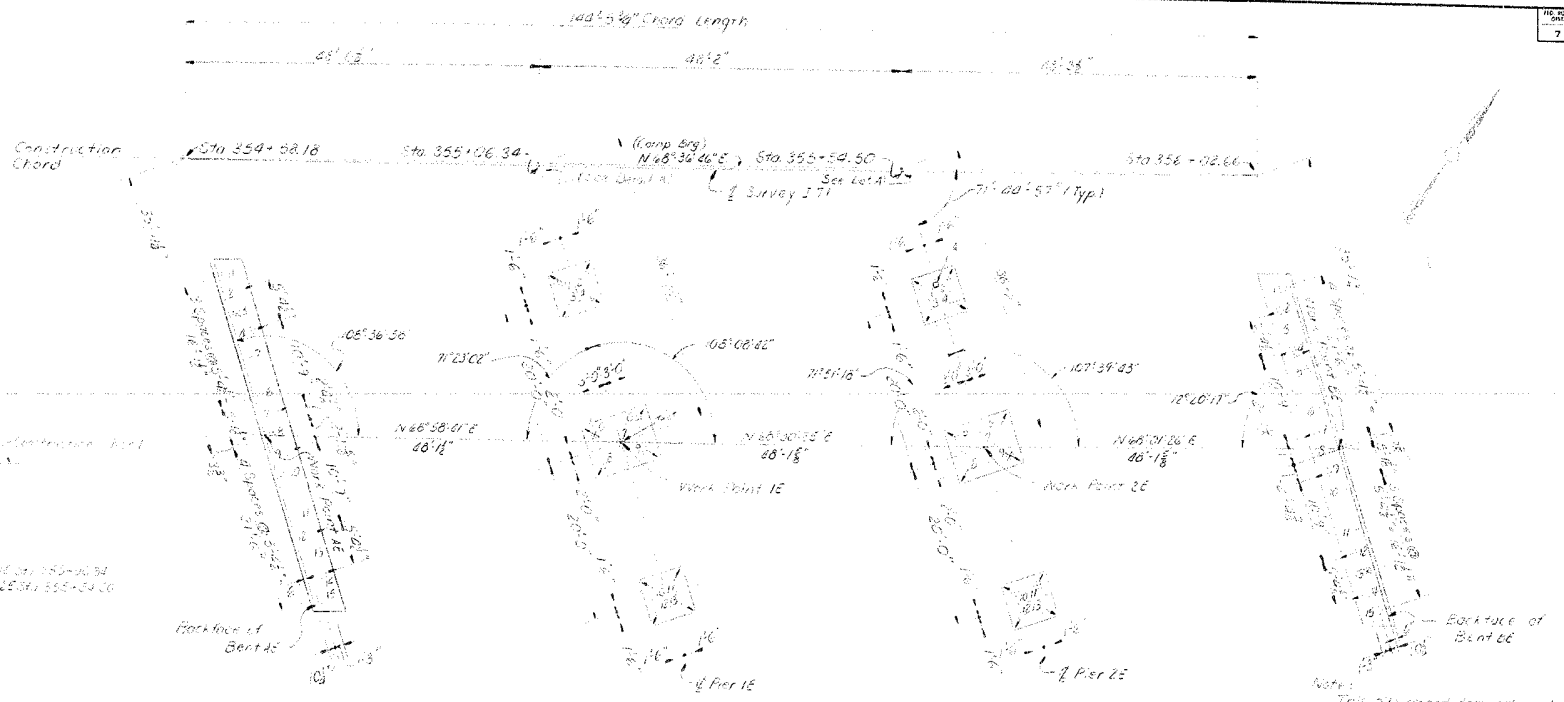
BRIDGE OVER FISCHER RAILROAD SPUR SHEET 13 OF 15

**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 FRANKFORT  
 COUNTY OF  
**JEFFERSON**  
 171 - OHIO STREET TO ZORN AVENUE  
 LOUISVILLE - COVINGTON  
 ROAD

STATION 355+33.19 PROJECT NO. 171

BRIDGE NUMBER 15936

BRIDGE



FILING PLAN - EASTBOUND BRIDGE

END BENT 15				
Pile No.	Cut-off Elevation	Tip of Pile as Driven	Length of Pile in Place	Calculated Bearing Capacity Tons
1	456.602	389.432	77.17	85
25		402.577	56.15	85
26		402.192	57.42	85
27		402.417	56.67	85
28		402.022	58.58	85
29		404.07	52.53	85
30		406.112	51.49	85
31		405.852	50.75	85
32		403.876	52.73	85
33		402.102	54.50	85
34		402.922	53.68	85
35		401.048	55.06	85
36		402.102	54.40	85
37		404.947	51.67	85
38		456.502	40.68	85

PIER 15				
Pile No.	Cut-off Elevation	Tip of Pile as Driven	Length of Pile in Place	Calculated Bearing Capacity Tons
15	430.674	382.244	48.43	85
16		382.156	48.52	85
17		397.148	33.52	85
18	430.674	392.499	38.18	85
19	430.172	381.471	48.70	85
20		397.148	33.52	85
21		382.244	48.43	85
22		382.156	48.52	85
23	430.74	392.499	38.18	85
24	430.674	392.244	38.43	85
25		402.102	54.50	85
26		404.947	51.67	85
27	430.674	382.244	48.43	85

PIER 26				
Pile No.	Cut-off Elevation	Tip of Pile as Driven	Length of Pile in Place	Calculated Bearing Capacity Tons
15	430.711	382.244	48.47	100
16		382.156	48.56	100
17		397.148	33.52	100
18	430.711	392.499	38.21	100
19	430.211	381.471	49.74	100
20		397.148	33.52	100
21		382.244	48.47	100
22		382.156	48.56	100
23	430.711	392.499	38.21	100
24	430.711	392.244	38.47	100
25		402.102	54.50	100
26		404.947	51.67	100
27	430.711	382.244	48.47	100

END BENT 26				
Pile No.	Cut-off Elevation	Tip of Pile as Driven	Length of Pile in Place	Calculated Bearing Capacity Tons
15	430.714	382.244	48.47	100
16		382.156	48.56	100
17		397.148	33.52	100
18	430.714	392.499	38.21	100
19	430.214	381.471	49.74	100
20		397.148	33.52	100
21		382.244	48.47	100
22		382.156	48.56	100
23	430.714	392.499	38.21	100
24	430.714	392.244	38.47	100
25		402.102	54.50	100
26		404.947	51.67	100
27	430.714	382.244	48.47	100

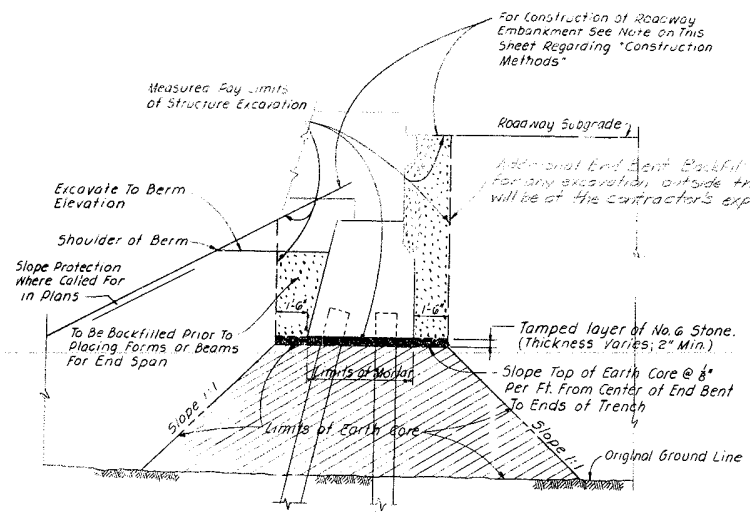
→ indicates direction of pile batter  
 \* indicates Test Pile for length  
 (Estimated length: 35' @ Piers & 60' @ End Bents)

BRIDGE OVER FISCHER RAILROAD SPUR SHEET 14 OF 15  
**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 FRANKFORT  
 COUNTY OF  
**JEFFERSON**  
 171 - OHIO STREET TO ZORN AVENUE  
 LOUISVILLE-COVINGTON  
 ROAD

EASTBOUND BRIDGE  
 PILE RECORD

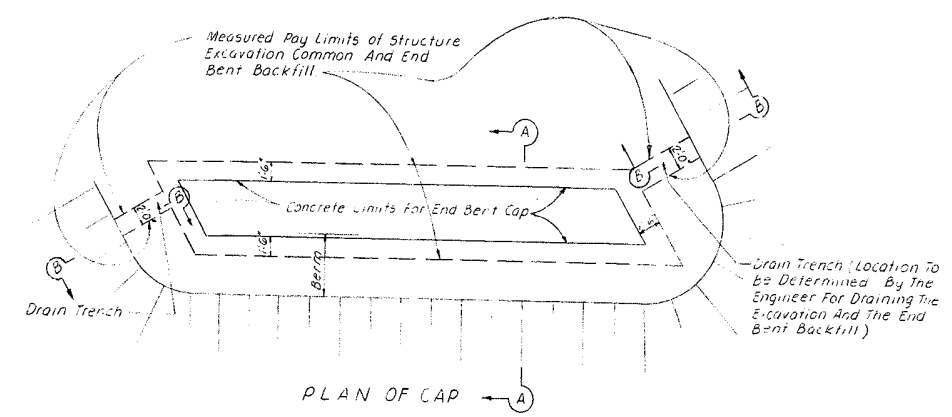
STATION 355+33.19	PROJECT NO. 171
BRIDGE NUMBER	DRAWING NO. 15936

HAZELT & ERDAL  
CONSULTING ENGINEERS  
FILE NO. 871-E

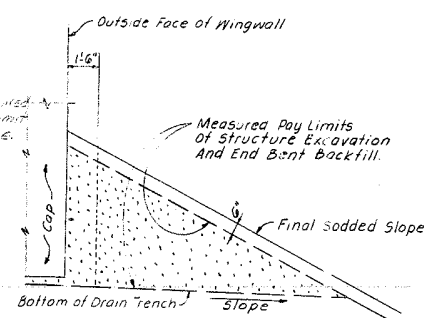


SECTION A-A

Legend: Pay limits for End Bent Backfill.



PLAN OF CAP



SECTION B-B  
(Thru Trench)

**NOTES**

**MATERIALS:** EARTH CORE-In Addition To Meeting Requirements For Embankment Materials in Accordance With The Specifications, The Earth Core Shall Be Free of Boulders or Any Other Obstructions Which Would Interfere With The Driving of Piles.

**END BENT BACKFILL:** End Bent Backfill Material Shall Consist of Not Sand or Concrete Sand or Dense Graded Aggregate Meeting Requirements of The Specifications; or Sand, Crushed or Uncrushed Gravel, Crushed Limestone, Crushed Sandstone, Crushed Slag or a Combination Thereof, Meeting The Following Requirements:

GRADATION	SIEVE SIZE	Percent Passing
	3/8 Inch	100
	1/2 Inch	50-90
	3/4 Inch	20-40

**SOUNDNESS:** Percentage Loss After Subjection To Five Alternations of The Sodium Sulfate Soundness Test, not More Than 20.

**WEAR SHALE:** Percent, Not More Than 5.

**DIRT:** Percent, Not More Than 5.

**CONSTRUCTION METHODS:** Prior To Driving Piles, The Roadway Embankment At The Location For Pile End Bent Shall Be Constructed To The Roadway Subgrade Elevation. At Full Embankment Section For Front And Side Slopes in Accordance With The Specifications For "Extra Compaction". After The Embankment Has Been Constructed, Excavate To The Bottom of Bent Cap Elevation Within The Limits Indicated Before Driving Piles. After Piles Are Driven (See General Notes) Slope Bottom of Excavation Toward Ends of Trench As Noted For Drainage. Place # 6 Crushed Stone in Trench To Bottom of End Bent Elevation, Tamped Well. Place 1:2 Cement Mortar To Bottom of Cap Elevation, Side Form For End Bent May Be Placed As Soon As Mortar Has Set Sufficient To Support Workmen And Forms Without Being Disturbed. After Concrete Cap Has Been Placed The Excavation Shall Be Filled With "End Bent Backfill" Material Noted Above To Level of The Berm Prior To Placing Beams For The Bridge. After Concrete Backwall Has Been Placed The "End Bent Backfill" Shall Be Placed Up To Subgrade Elevation. If The Original Excavation Has Been Enlarged Beyond The Limits Shown, The Entire Excavation, Regardless of The Limits Shall Be Backfilled With The End Bent Backfill Material.

End Bent Backfill Shall Be Placed in Drainage Trenches At Ends of Excavation To Embankment Side Slopes, As Shown in Section B-B. The Backfill Shall Be Tamped By Hand Tampers, Pneumatic Tampers or Other Means Approved By The Engineer. Care Shall Be Exercised To Thoroughly Compact The Backfill Under The Haunches of The Structure To Insure That The Backfill is in Intimate Contact With The Sides of The Structure. The Density of The Backfill Shall Be At Least Equal To That Required For The Adjacent Embankment.

**MEASUREMENT AND PAYMENT:** Excavation For The Pile End Bent Shall Be Measured As Shown in "Plan" And "Section" And Will Be Paid For As "Structure Excavation Common" in Accordance With The Specifications. Pay Limits For Structure Excavation Shall Serve As Limits For Determining Quantity For Payment For "End Bent Backfill" Which Quantity Shall Be Paid For At The Unit Price Per Cubic Yard Bid For End Bent Backfill, except that the volume of concrete cap-bent excavation shall be deducted from the measurement for "Structure Excavation" to establish the quantity for payment for "End Bent Backfill". Crushed stone and mortar placed under the end bent will not be paid for separately and shall be incidental to and included in the "Unit Price Bid for Class "A" concrete.

BRIDGE LARSEN 171 OVER FISHES SPRING R.R. SHEET 15 OF 15

**COMMONWEALTH OF KENTUCKY**  
DEPARTMENT OF HIGHWAYS  
FRANKFORT  
COUNTY OF  
**JEFFERSON**  
171 - OHIO STREET TO ZORN AVENUE  
LOUISVILLE - COVINGTON  
ROAD

STATION 155+3.19  
BRIDGE NUMBER 15036  
PROJECT NO. SP56-33-L2  
171-16100

END BENT BACKFILL

**BRIDGE**